

RICHMONT METROPOLITAN TRANSPORTATION AUTHORITY

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RICHMONT EXPRESSWAY SYSTEM

CONTRACT NO. MR – 2018

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ADDENDUM NO. 3

February 28, 2018

DESCRIPTION

The attached pages are issued to amend, and become part of the Contract Documents.

Contractor shall acknowledge receipt of this Addendum on the Receipt of Addenda form (Contract Page RA-1). Failure to complete and include the RA-1 form in the bid package may cause bid to be found irregular.

There is no change in the date that bids are due, and no change in the contract completion date from the dates contained in the Invitation to Bid.

**SP-I-2 Bridge Details**

All Bidders are directed to replace Page SP-I-2 from the Bid Documents with Addendum No. 3 Page SP-I-2.

This Addendum has been issued to revise the quantities in Special Provision to match the repair detail.

**Boulevard Bridge Truss Sketches**

All Bidders are directed to add the following Boulevard Bridge truss sketches showing details on the type and location of work required as part of this contract.

All Bidders are directed to add Pages Addendum No. 3 SP-I-6a thru SP-I-6v.

ADDENDUM NO. 3  
2/28/2018

Prior to any steel fabrication, the Contractor shall field verify all dimensions and assess the working conditions to determine any constructability issues. Should the Contractor have any issues or questions, they shall be submitted to the Engineer prior to steel fabrication and start of work.

The Kanawha Canal discharges into the James River which is in the Chesapeake Bay Watershed and all work may be subject to the provisions in the Chesapeake Bay Preservation Act; therefore, no debris or wastewater of any type shall be discharged into the canal or river. Furthermore, the Contractor shall be responsible for compliance with all environmental laws and regulations regarding this type of work. All environmental permits as well as submittals, if required, shall be incidental to this work.

The Contractor is advised that the area under the Boulevard Bridge and Bridge 67 is not owned by RMTA and may require work permits from the City of Richmond.

For lane closures on and underneath these bridges and associated measurement and payment items, the Contractor shall refer to the special provisions for Maintenance of Traffic.

## **BRIDGE DESCRIPTIONS**

### Boulevard Bridge

The Boulevard Bridge is a two-lane bridge that carries State Route 161 (Westover Hills Boulevard) over the James River as well as Norfolk Southern and CSX Railroads. The superstructure consists of 13 spans of a semi-continuous steel girder floor beam system and 11 spans of a semi-continuous steel deck truss. The bridge is a weight limited structure; the maximum weight of a vehicle is 7,500 pounds. The repairs on Boulevard bridge includes;

The fabrication of three different types of lacing bars (250 each of Type A, 30 each of Type B and 30 each of Type C) and procuring a total of 670 bolts/nuts/washers.

The removal of damaged lacing bars and rivets and replacement with a new lacing bar and bolts at various locations.

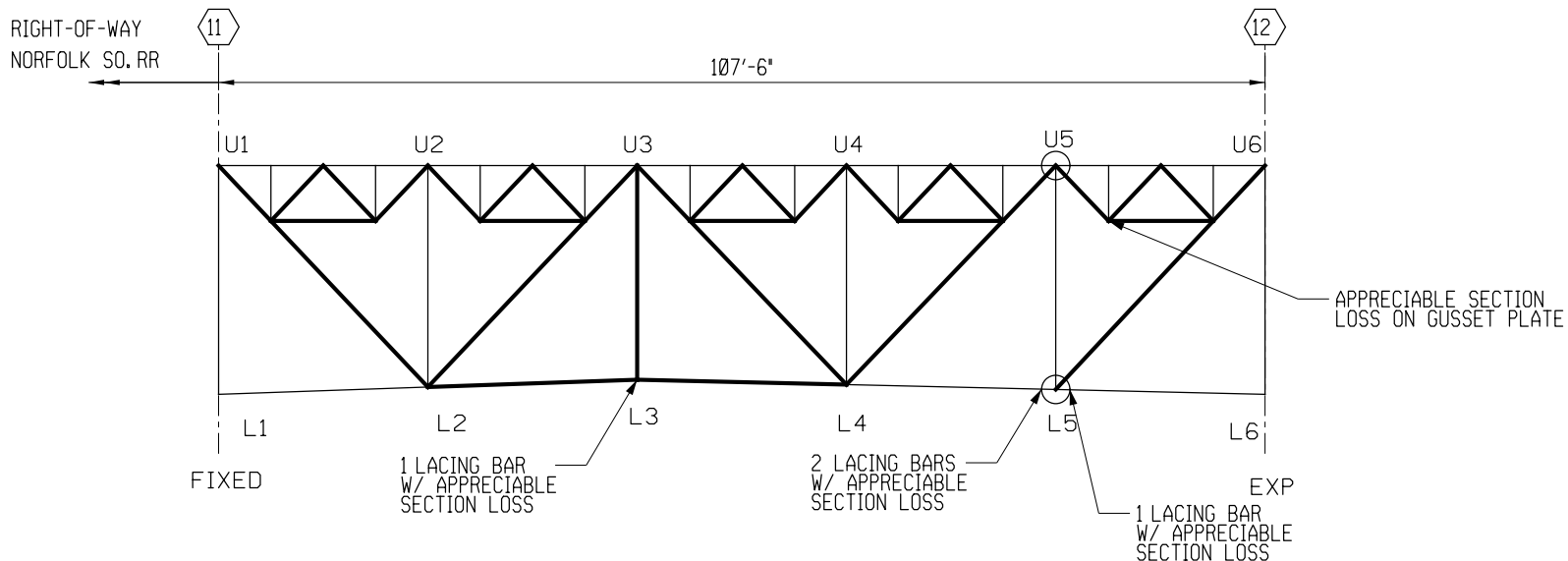
The removal of damaged or missing rivets and replacement with new bolts at various locations.

The fabrication of 30 batten plates and rivets.

The removal of the damaged batten plates and missing rivets and replacement with a new batten plates and bolts at various location.

The removal of 3' end of the flat bracing plate and installing proposed replacement plate length between the angles bolting through existing holes.

Addendum -3  
SP-I-6a



EAST TRUSS ELEVATION  
SPAN 5  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

Richmond Metropolitan Transportation Authority

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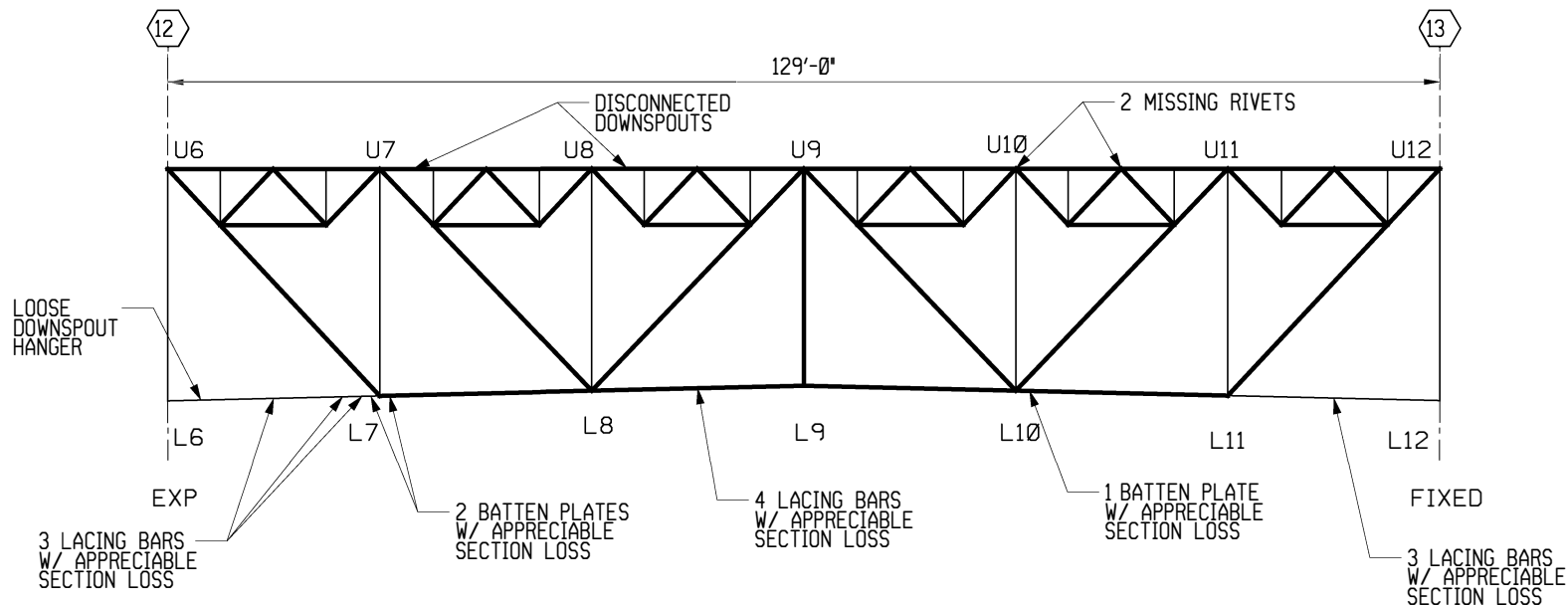
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RMTA BOULEVARD  
BRIDGE

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Addendum -3  
SP-I-6b



EAST TRUSS ELEVATION  
SPAN 6  
(LOOKING WEST)  
SCALE: 1"=20'

⬡ - PIER NUMBER  
○ - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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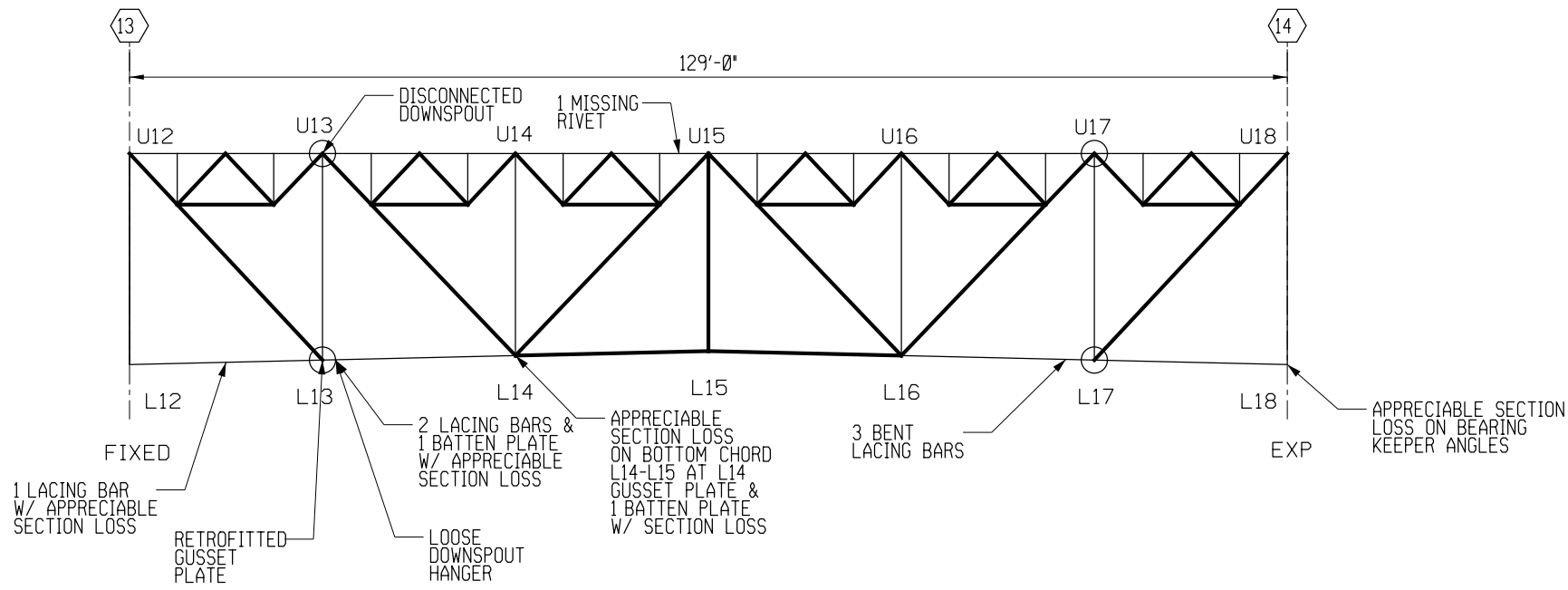
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Addendum -3  
SP-I-6c



EAST TRUSS ELEVATION  
SPAN 7  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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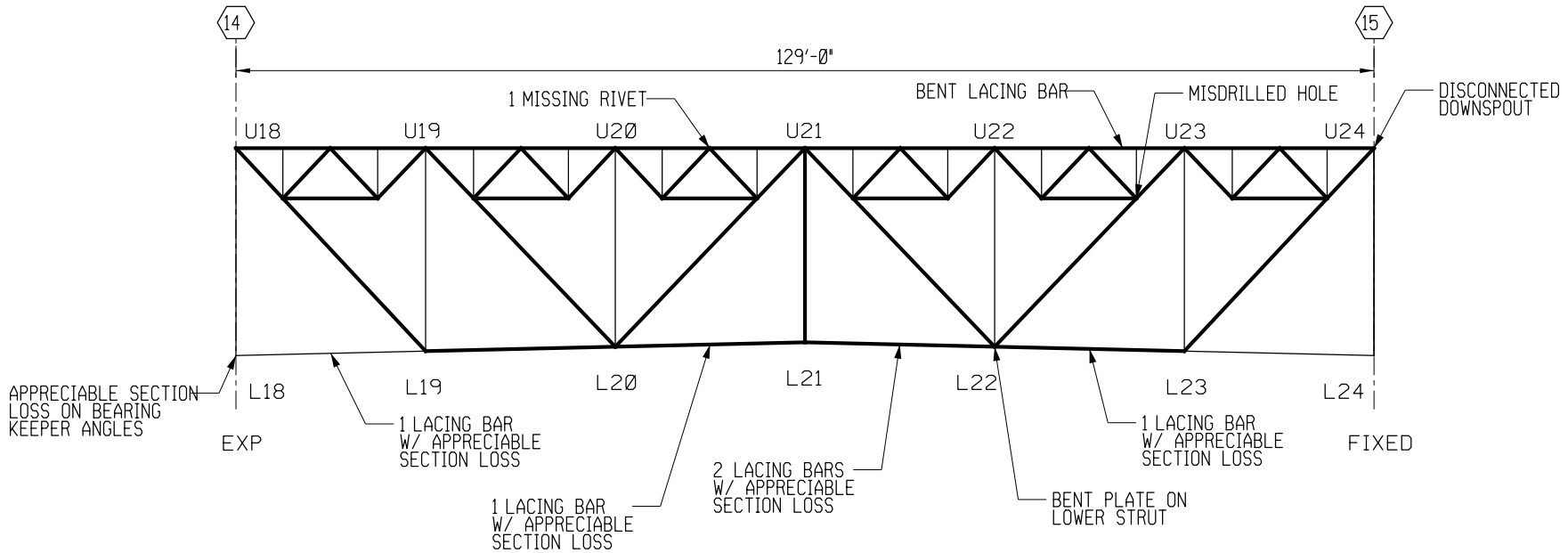
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SP-I-6d



EAST TRUSS ELEVATION  
SPAN 8  
(LOOKING WEST)  
SCALE: 1"=20'

- ⬡ - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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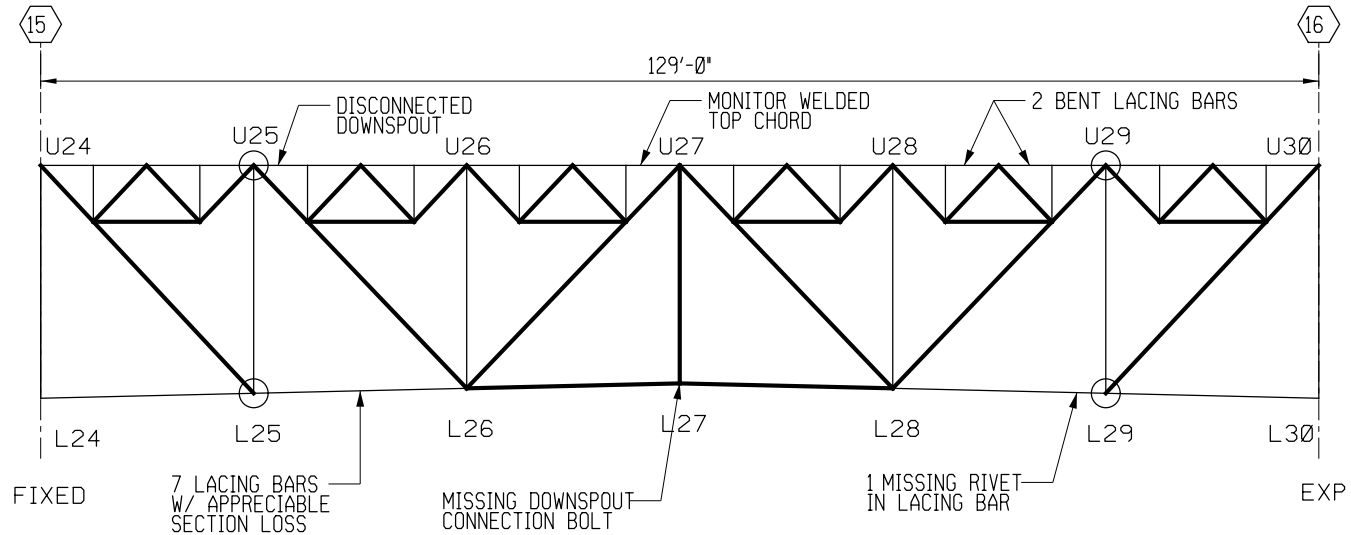
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SP-I-6e



EAST TRUSS ELEVATION  
SPAN 9  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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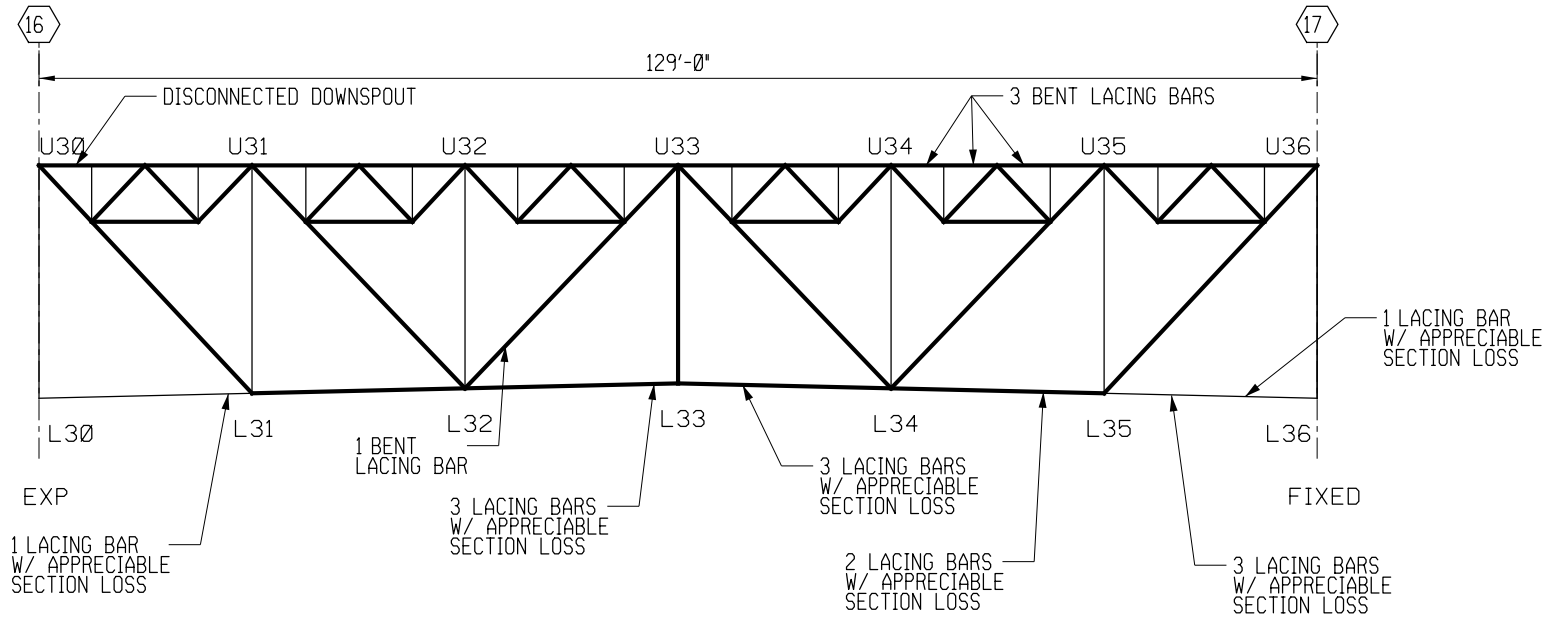
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Addendum -3  
SP-I-6f



EAST TRUSS ELEVATION  
SPAN 10  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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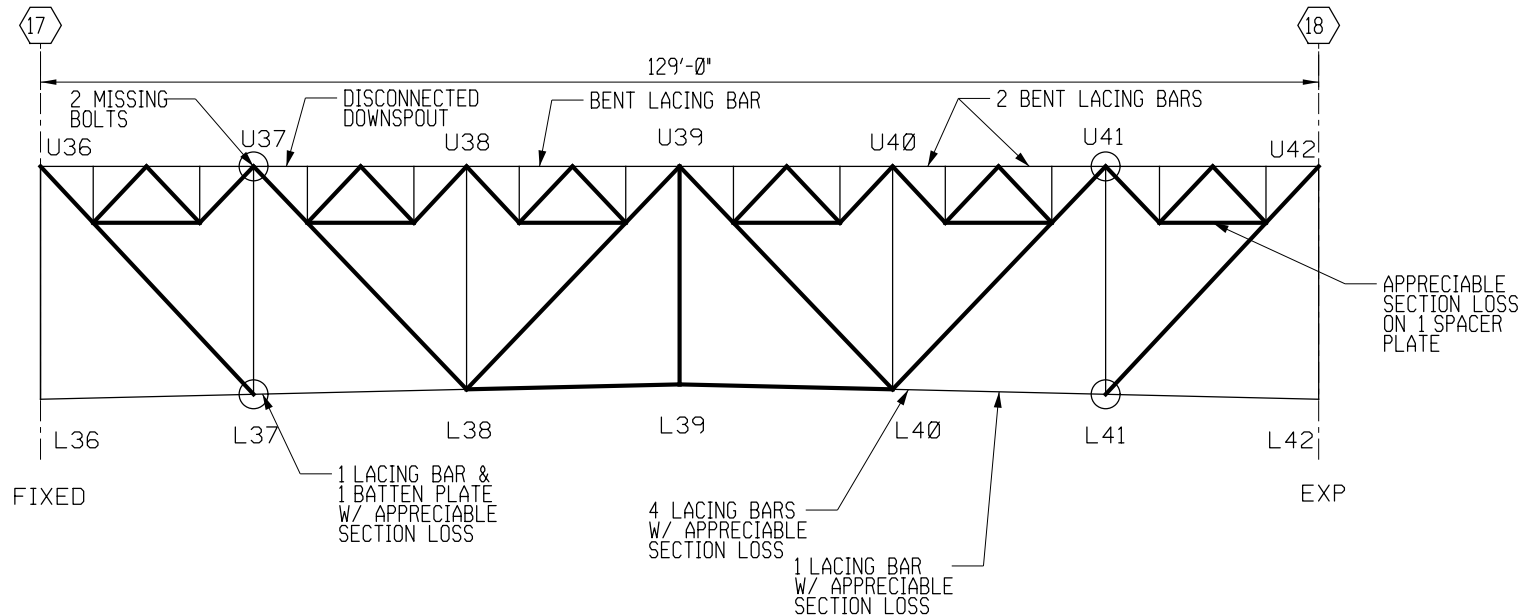
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Addendum -3  
SP-I-6g



EAST TRUSS ELEVATION  
SPAN 11  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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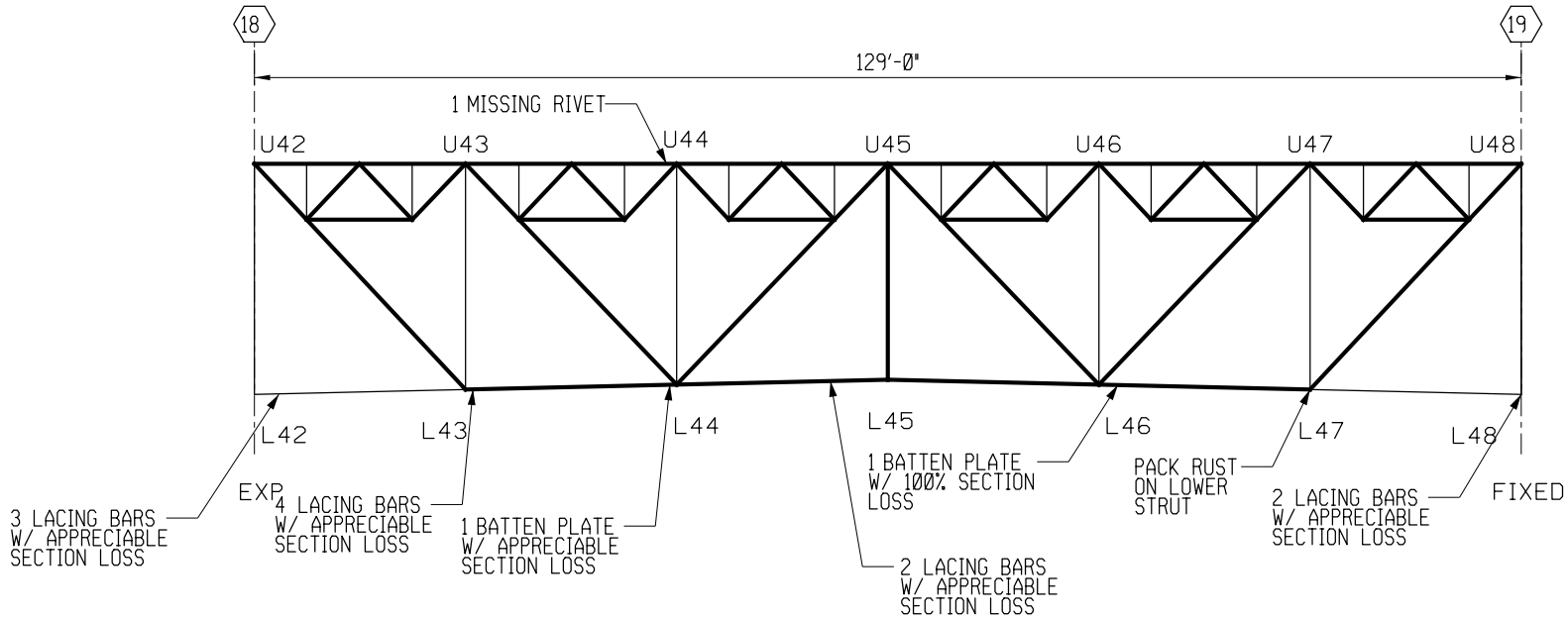
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SP-I-6h



EAST TRUSS ELEVATION  
SPAN 12  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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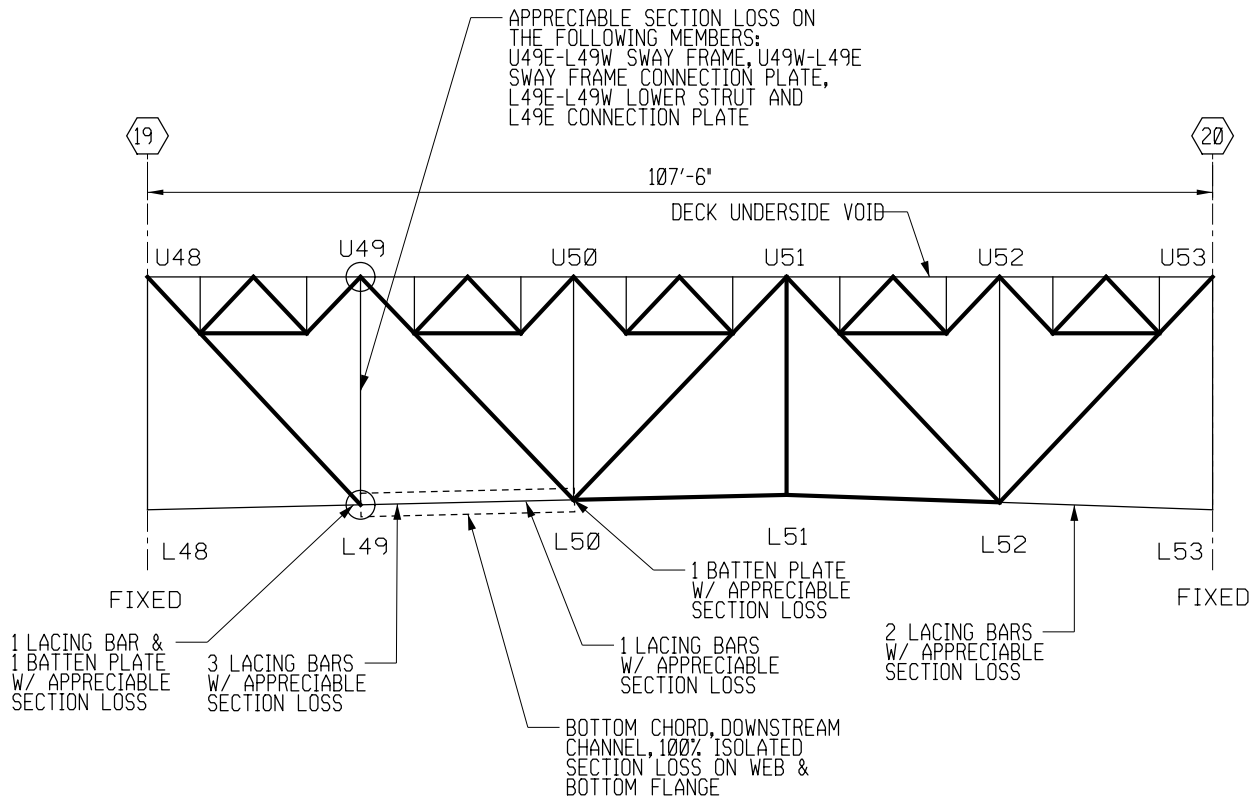
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**EAST TRUSS ELEVATION**  
**SPAN 13**  
 (LOOKING WEST)  
 SCALE: 1"=20'

# - PIER NUMBER  
 ○ - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

Addendum -3  
 SP-I-6i

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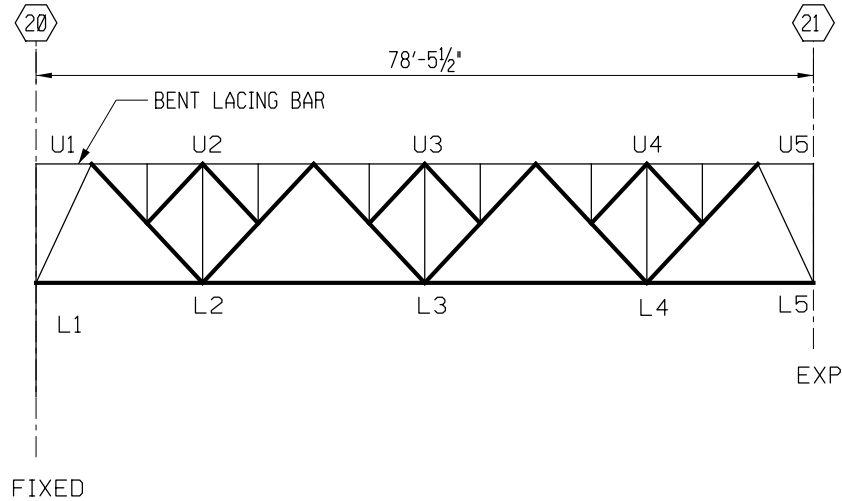


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Addendum -3  
SP-I-6j



EAST TRUSS ELEVATION  
SPAN 14  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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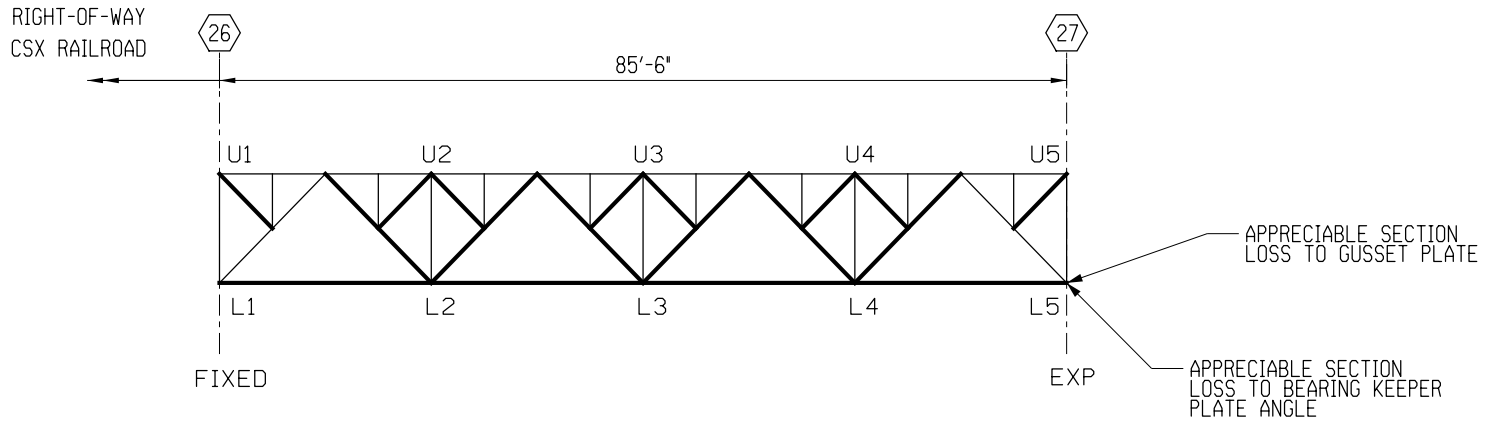
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SP-I-6k



EAST TRUSS ELEVATION  
SPAN 20  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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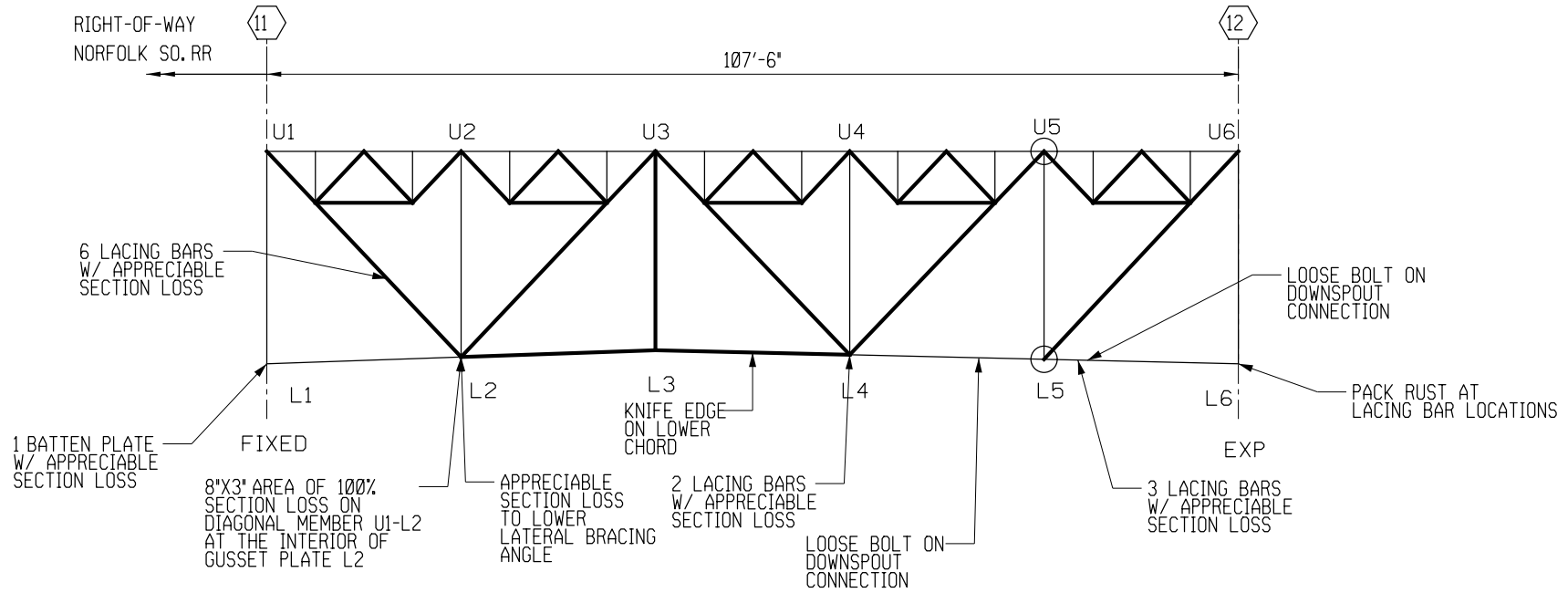
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SP-I-61



WEST TRUSS ELEVATION  
SPAN 5  
(LOOKING WEST)  
SCALE: 1"=20'

- ⬡ - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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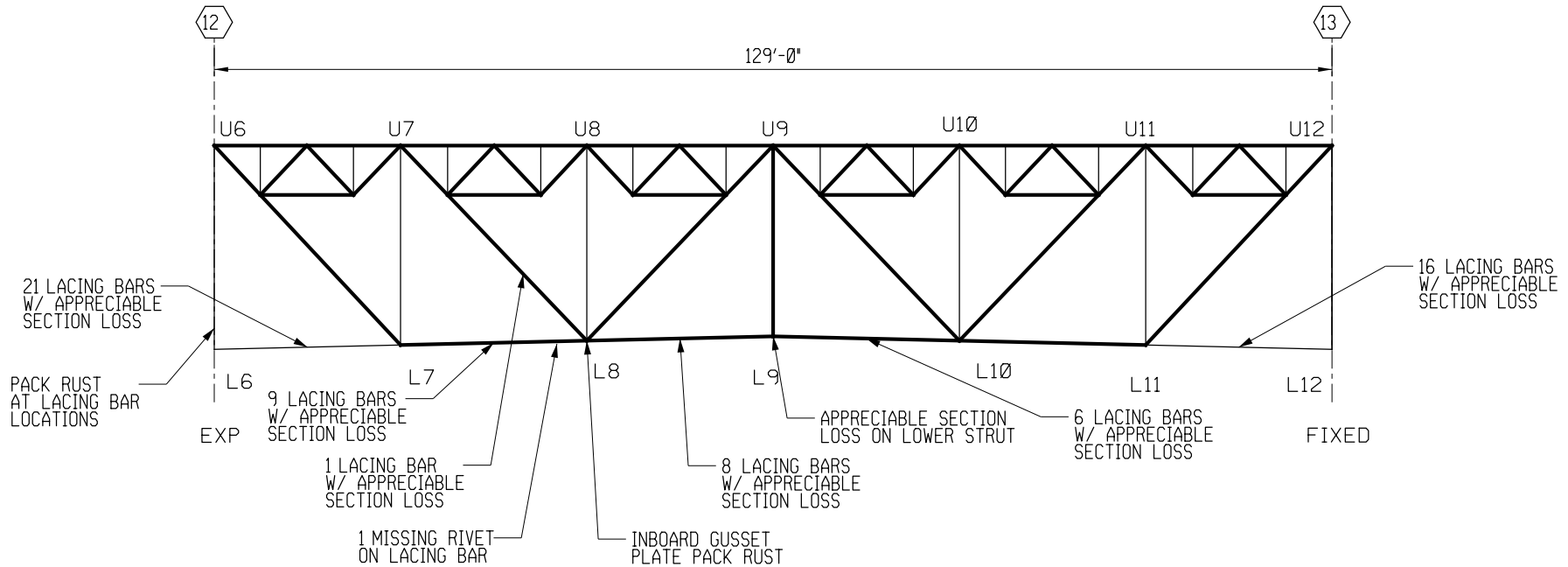
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SP-I-6m



WEST TRUSS ELEVATION  
SPAN 6  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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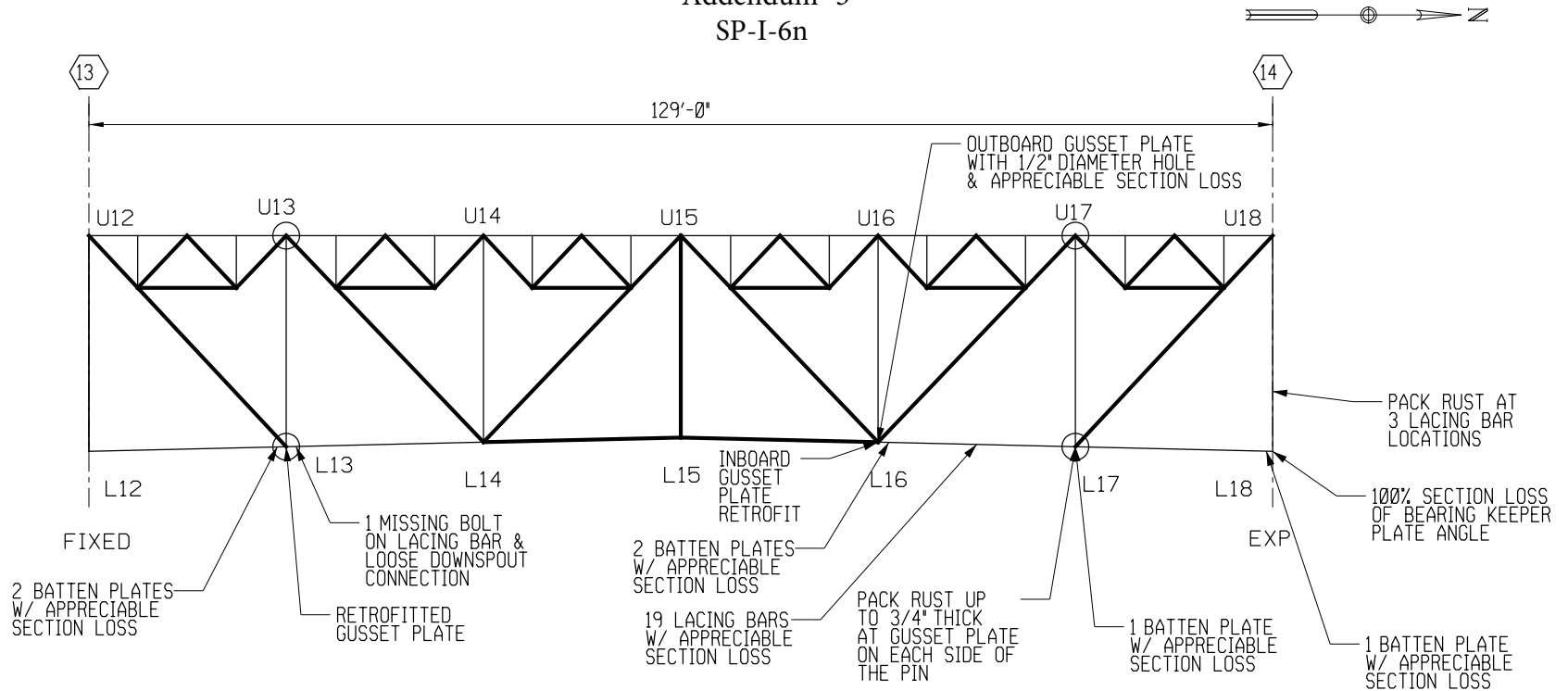
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SP-I-6n



**WEST TRUSS ELEVATION**  
**SPAN 7**  
(LOOKING WEST)  
SCALE: 1"=20'

# - PIER NUMBER  
○ - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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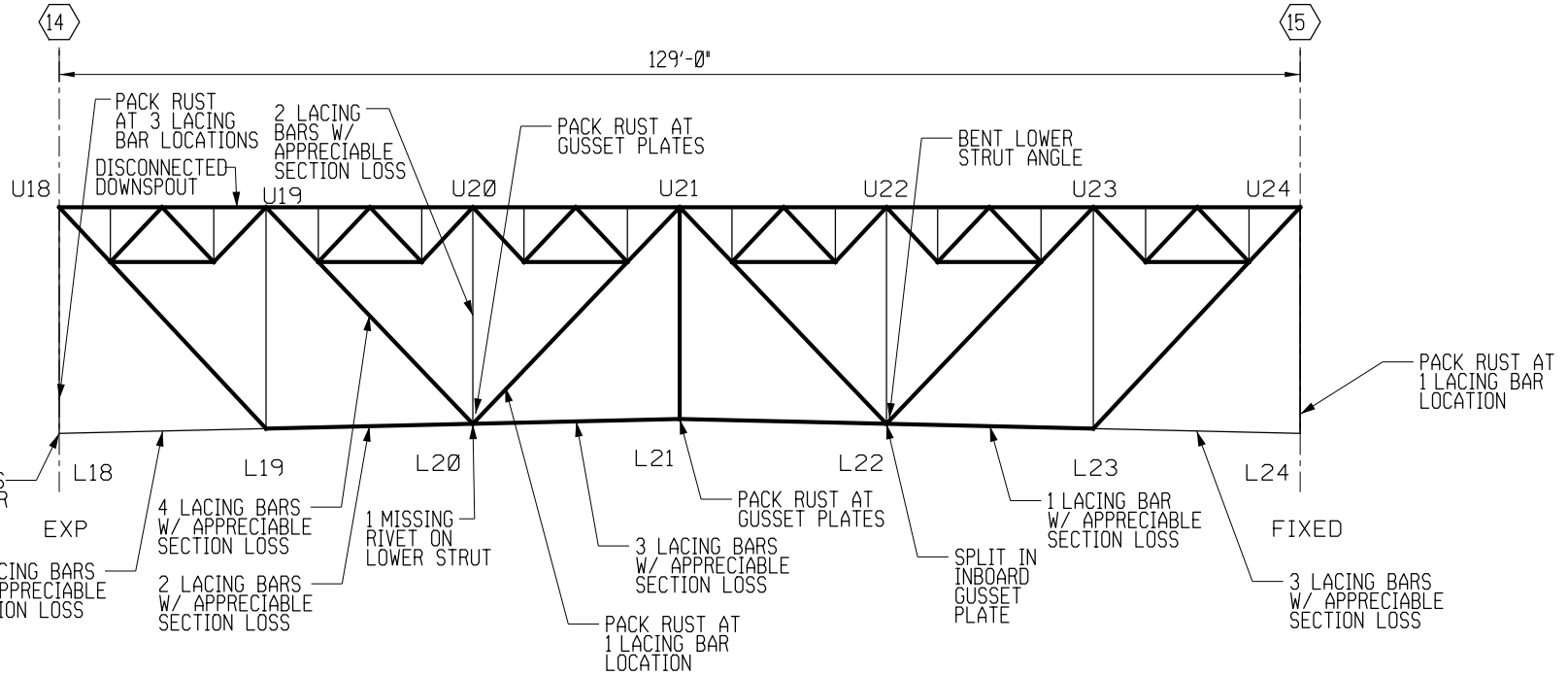
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SP-I-60



WEST TRUSS ELEVATION  
SPAN 8  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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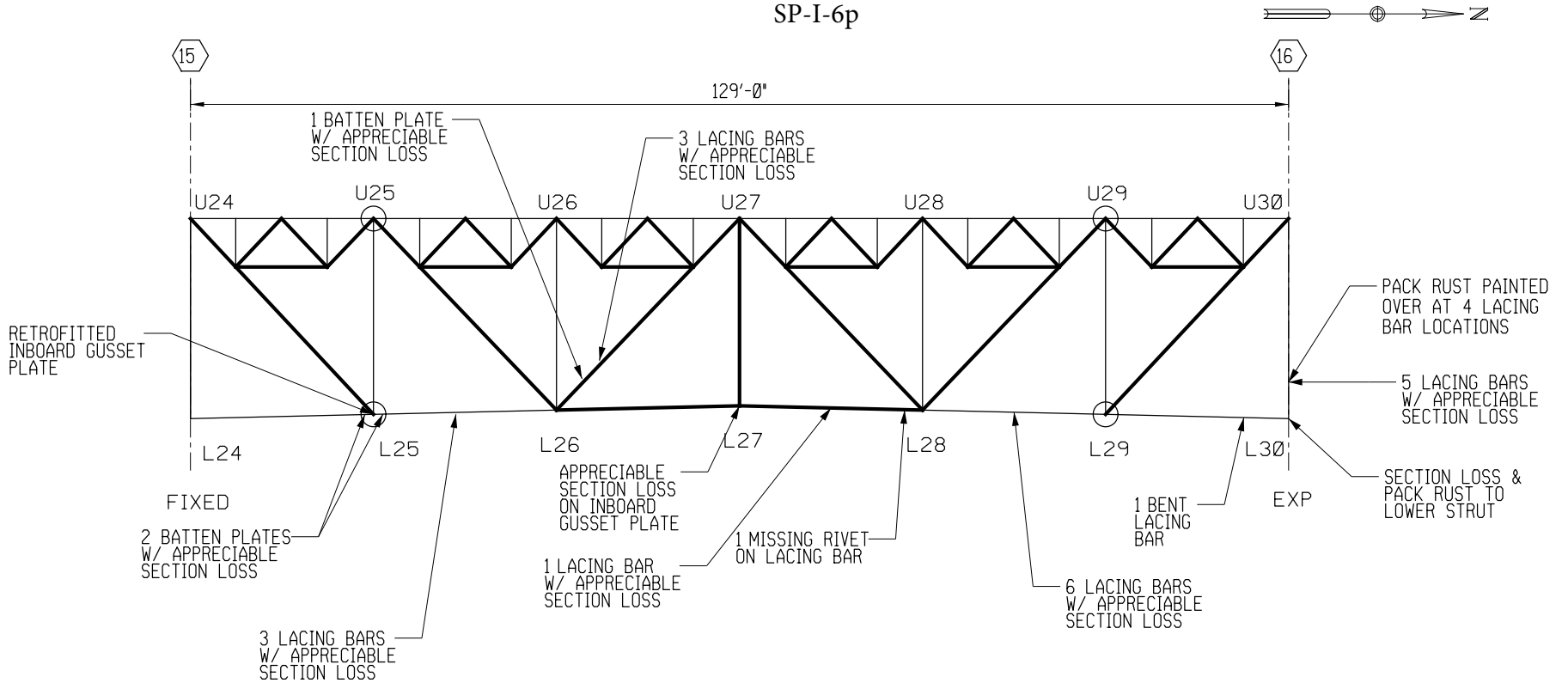
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SP-I-6p



WEST TRUSS ELEVATION  
SPAN 9  
(LOOKING WEST)  
SCALE: 1"=20'

# - PIER NUMBER  
○ - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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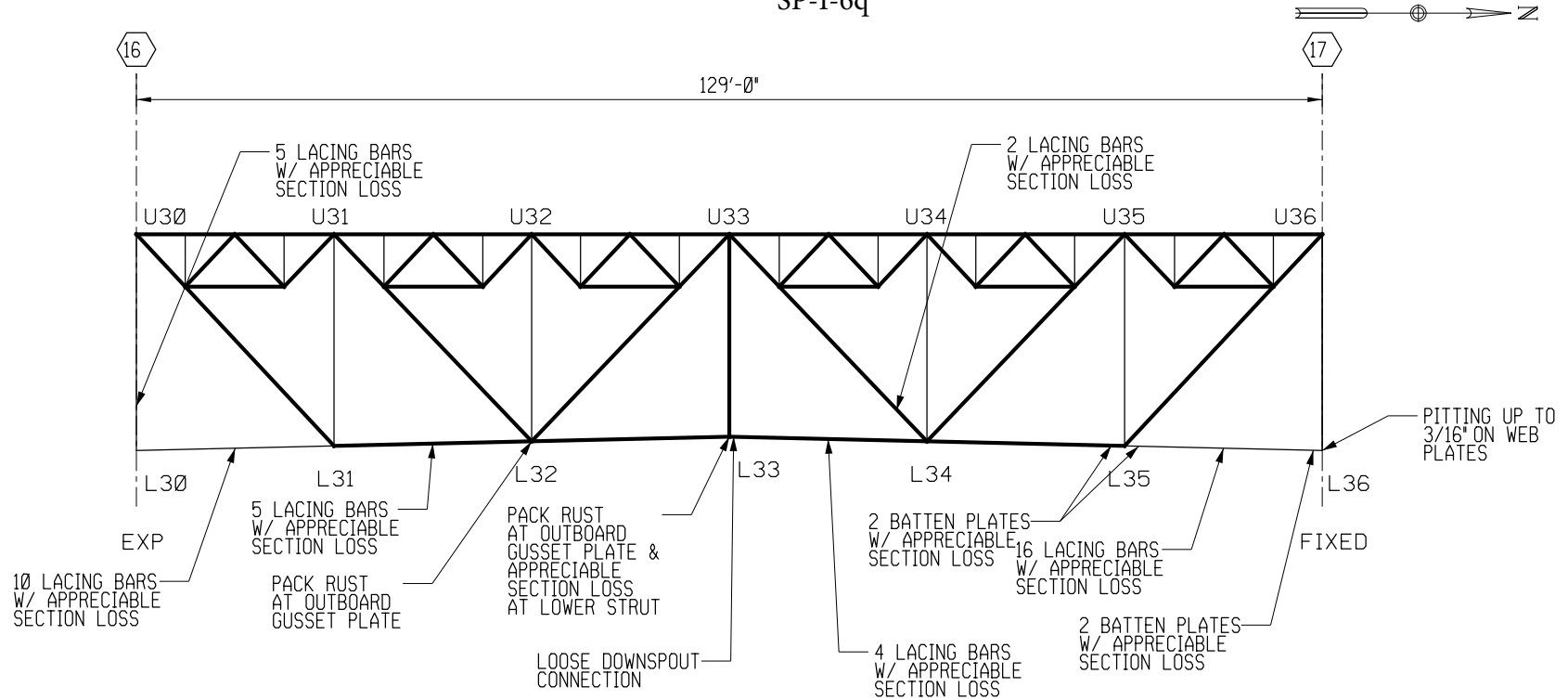
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SP-I-6q



**WEST TRUSS ELEVATION**  
**SPAN 10**  
(LOOKING WEST)  
SCALE: 1"=20'

# - PIER NUMBER  
○ - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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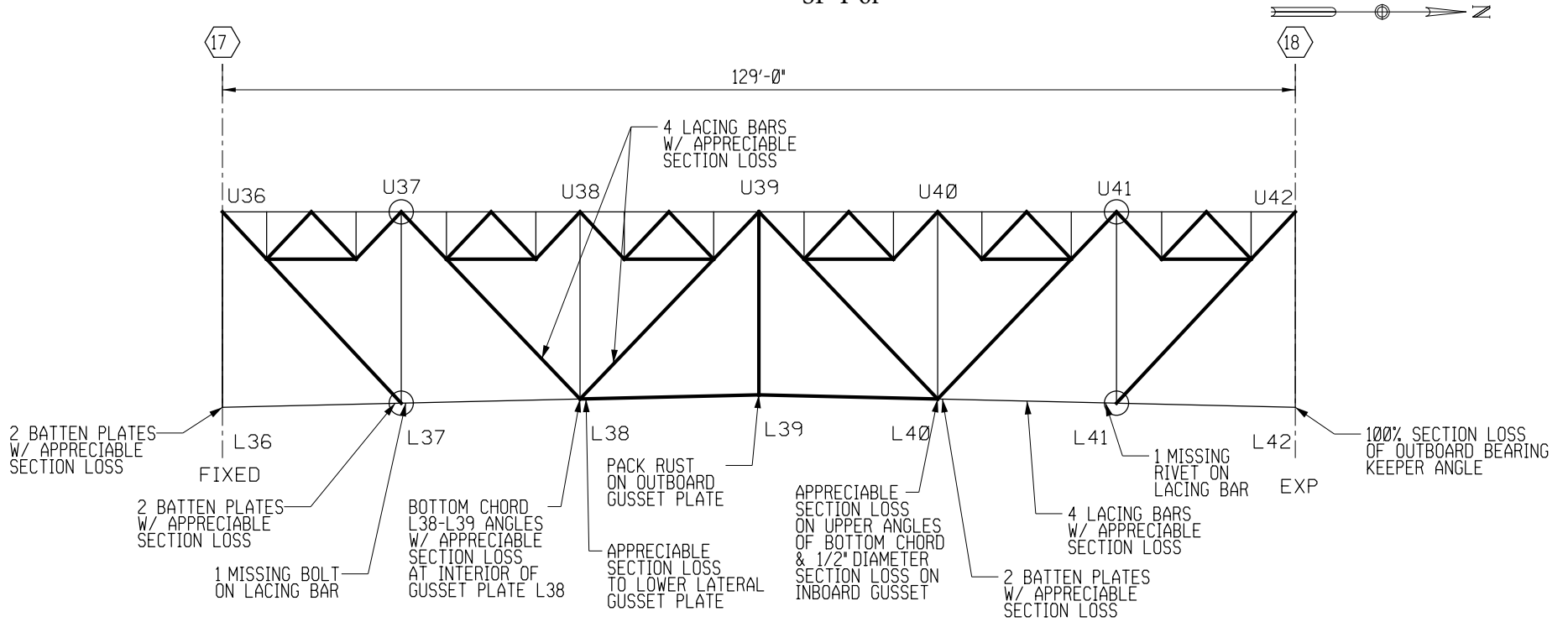
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SP-I-6r



WEST TRUSS ELEVATION  
SPAN 11  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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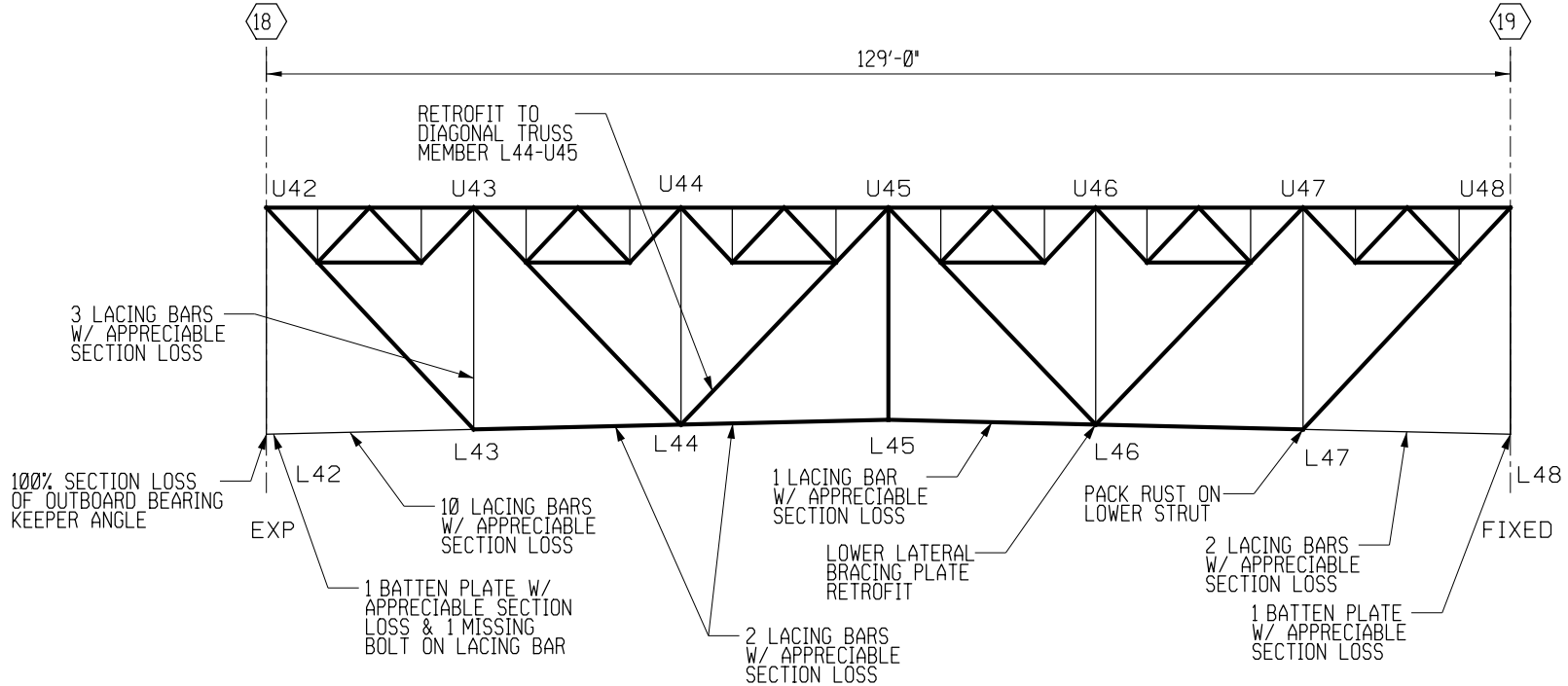
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SP-I-6s



**WEST TRUSS ELEVATION**  
**SPAN 12**  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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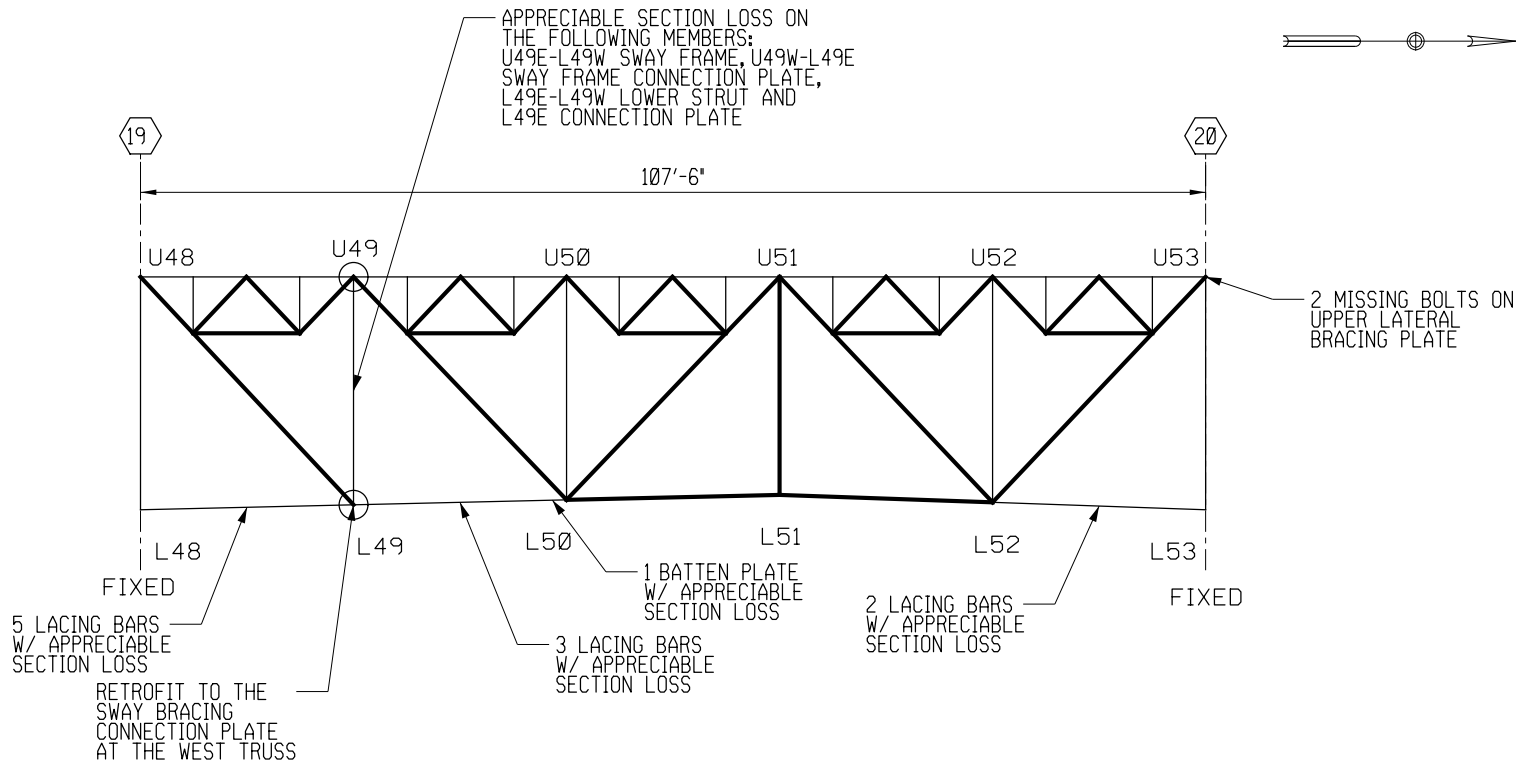
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**WEST TRUSS ELEVATION**  
**SPAN 13**  
 (LOOKING WEST)  
 SCALE: 1"=20'

# - PIER NUMBER  
 ○ - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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 SP-I-6t

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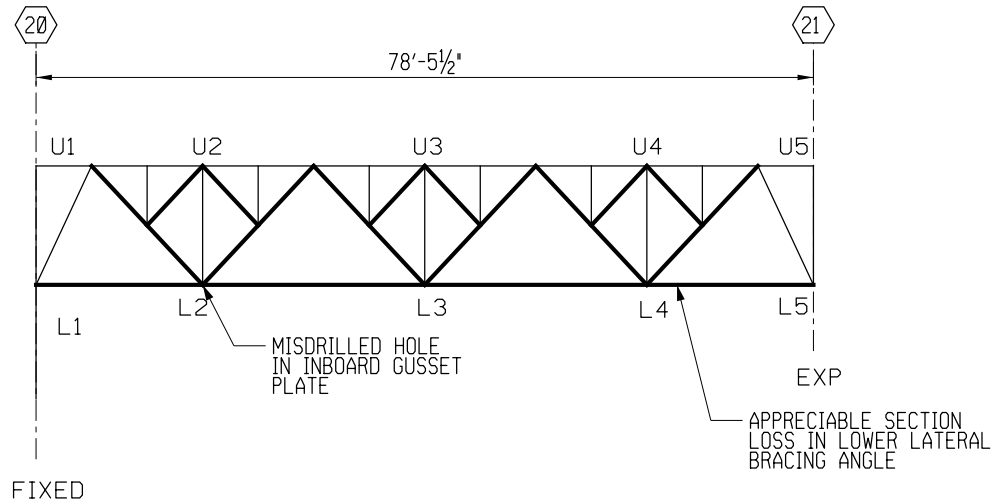
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SP-I-6u



WEST TRUSS ELEVATION  
SPAN 14  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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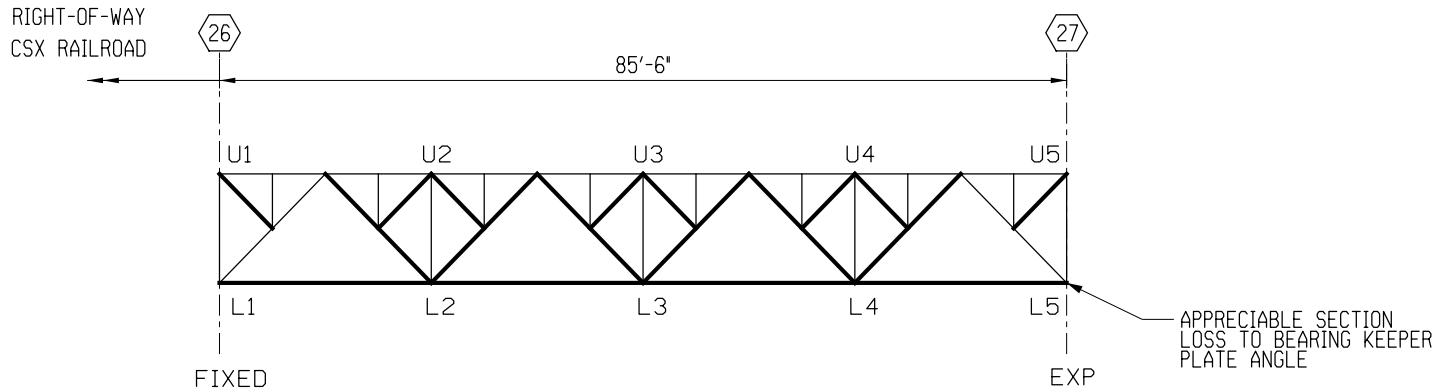
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Addendum -3  
SP-I-6v



WEST TRUSS ELEVATION  
SPAN 20  
(LOOKING WEST)  
SCALE: 1"=20'

- # - PIER NUMBER
- - FRACTURE CRITICAL PIN

NOTE: TRUSS MEMBERS SHOWN IN BOLD DENOTE FRACTURE CRITICAL MEMBERS.

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